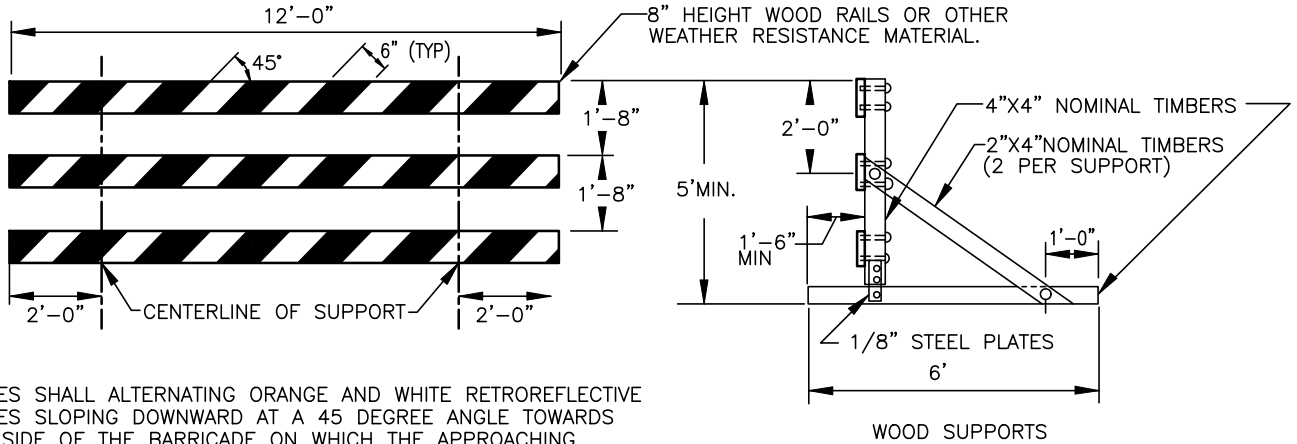
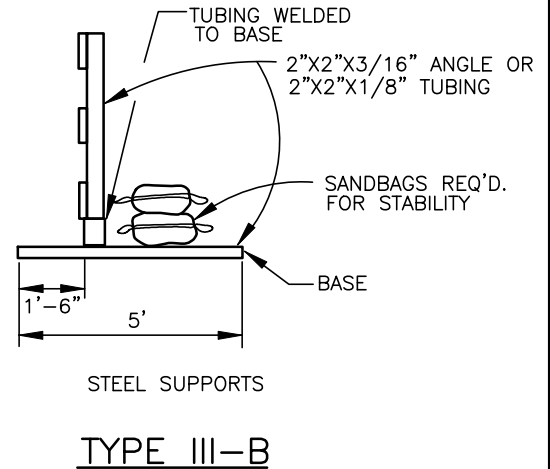
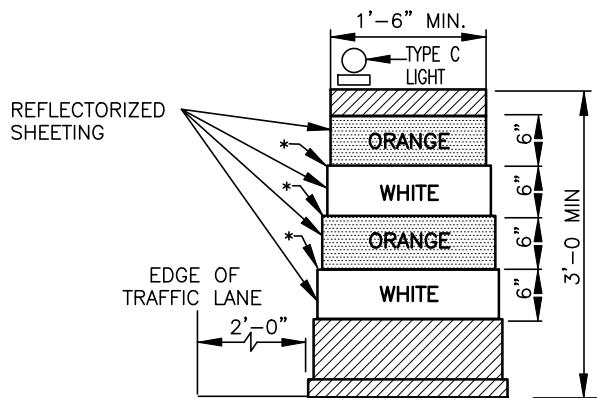


NOTE:
USE TYPE 'B' HIGH INTENSITY FLASHING YELLOW LIGHTS AS REQ'D.



NOTE:
STRIPES SHALL ALTERNATING ORANGE AND WHITE RETROREFLECTIVE STRIPES SLOPING DOWNWARD AT A 45 DEGREE ANGLE TOWARDS THAT SIDE OF THE BARRICADE ON WHICH THE APPROACHING VEHICLE MUST PASS. (STRIPES AS SHOWN INDICATE VEHICLE MUST PASS TO LEFT OF BARRICADE.)

TYPE III-B



CHANNELIZING DEVICE

* THE MAXIMUM DISTANCE BETWEEN THE EDGES OF ADJACENT REFLECTIVE SHEETING STRIPS SHALL BE 2 IN.

NOTES:

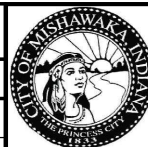
- THE SPACING OF CHANNELIZING DEVICES SHALL BE AS FOLLOWS:
 - WHERE POSTED SPEED LIMIT IS 50MPH OR GREATER THE SPACING SHALL BE 100FT.
 - WHERE POSTED SPEED LIMIT IS LESS THAN OR EQUAL TO 45MPH, THE SPACING SHALL BE 50FT
- SPACING OF CHANNELIZING DEVICES ON TAPERS SHALL BE NUMERICALLY EQUAL IN FEET TO THE POSTED SPEED LIMIT IN MPH.
- ALL TEMPORARY TRAFFIC CONTROL DEVICES SHALL BE MAINTAINED IN GOOD CONDITION AND MEET THE MINIMUM MAINTAINED RETROREFLECTIVITY LEVELS PER THE CURRENT INDIANA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

PREPARED BY: DLZ INDIANA, LLC



Brian M. Smith
2/1/2022

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CITY OF MISHAWAKA, INDIANA

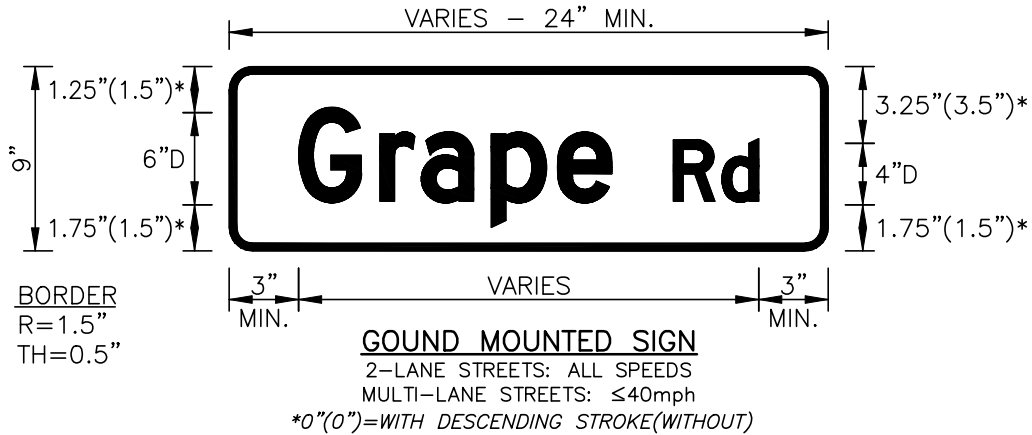
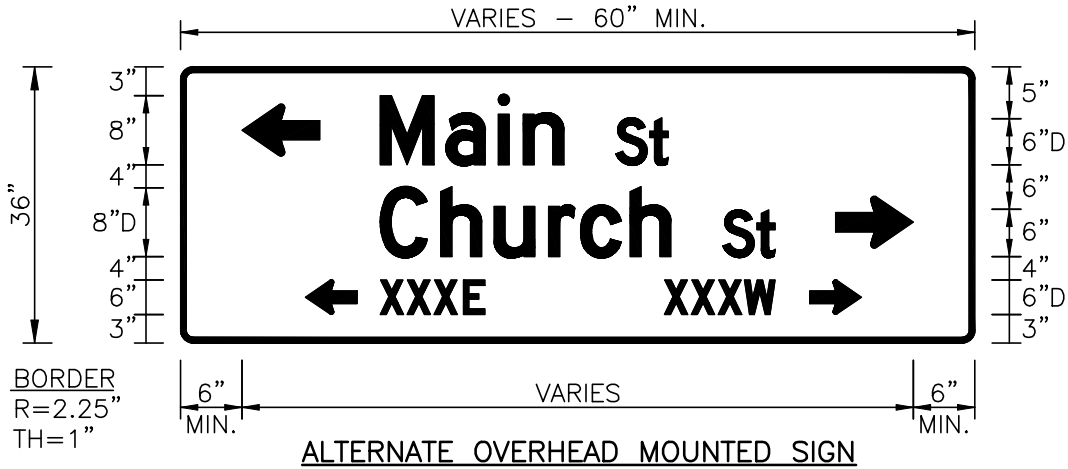
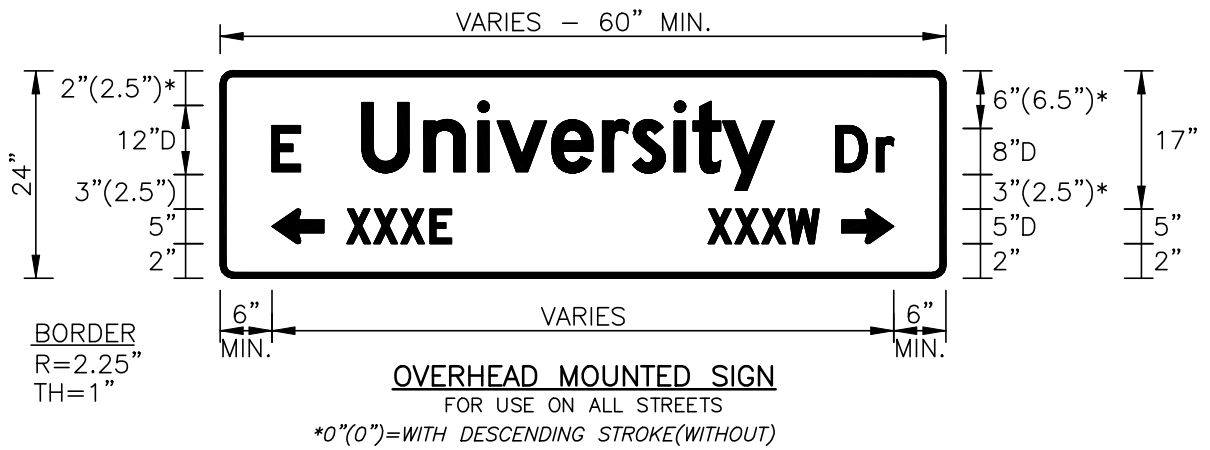
ENGINEERING STANDARDS

STANDARD BARRICADES

SHT. NO.

VIII-1

ITEM	REVISION	APPROVED DATE
	EFFECTIVE	APRIL 2007
	REVISED	MARCH 2013
	REVISED	FEBRUARY 2022



MATERIALS

SHEETING: PRISMATIC SHEETING TYPE VI OR GREATER
 BLANK: ALUMINUM, MIN. 0.125-IN THICK WITH ROUNDED CORNERS
 LETTERING: FHWA SERIES 'D' FONT, LOWER-CASE WITH INITIAL UPPER-CASE
 LETTERS: WHITE
 BACKGROUND: GREEN
 BORDER: WHITE

NOTES:

1. ALL ROAD SIGNS SHALL BE APPROVED BY THE ENGINEER

PREPARED BY: DLZ INDIANA, LLC

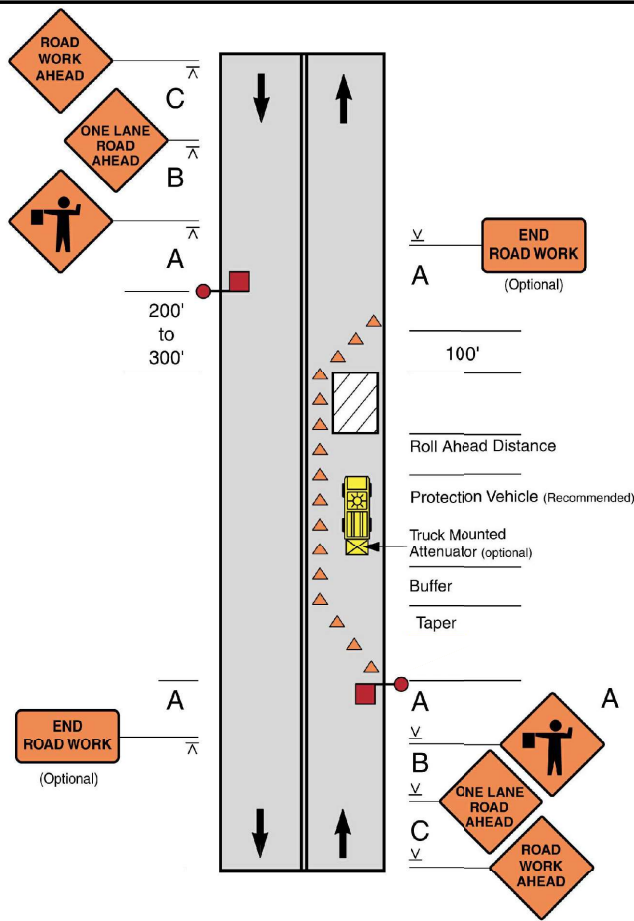
Brian M. Smith
2/1/2022

APPROVED/REVISED BY THE BOARD OF PUBLIC WORKS & SAFETY				CITY OF MISHAWAKA, INDIANA	
ITEM	REVISION	APPROVED DATE		ENGINEERING STANDARDS	
	EFFECTIVE		APRIL 2007	STREET NAME SIGNS	
	REVISED		MARCH 2013		
	REVISED		FEBRUARY 2022		
				SHT. NO. VIII-2	

LANE CLOSURE ON A TWO-LANE ROAD

(TWO FLAGGER OPERATION)

SHORT TERM STATIONARY – 1 TO 12 HOURS



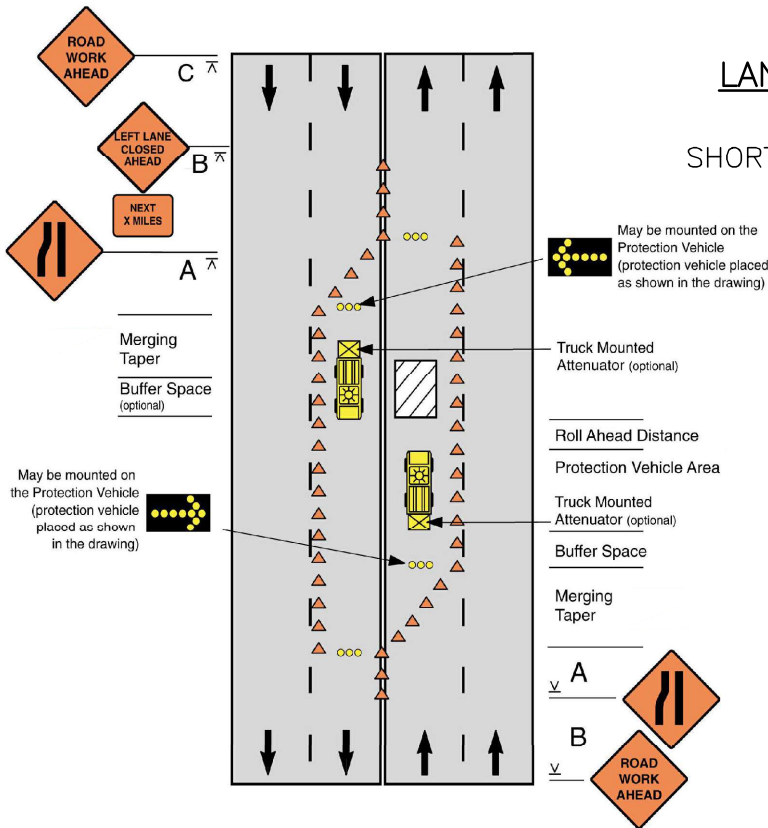
NOTES:

1. THE FLAGGER OR FLAGGERS SHALL USE APPROVED FLAGGING PROCEDURES ACCORDING TO THE MUTCD. IF THERE IS A SIDEROAD INTERSECTION WITHIN THE WORK AREA, ADDITIONAL TRAFFIC CONTROL, SUCH AS FLAGGERS AND APPROPRIATE SIGNAGE, MAY BE NEEDED ON THE SIDEROAD APPROACHES.

Speed Limit (mph)	Sign Spacing A (ft)	Sign Spacing B (ft)	Sign Spacing C (ft)	Buffer
25	200	200	200	55
30	200	200	200	85
35	350	350	350	120
40	350	350	350	170
45	500	500	500	220
50	500	500	500	280
55	500	500	500	335

LANE CLOSURE ON A FOUR-LANE UNDIVIDED ROAD

SHORT TERM STATIONARY – 1 TO 12 HOURS



TAPER LENGTH FORMULA:

SPEED LIMIT	TAPER LENGTH (FT)
40 MPH OR LESS	$L = (WS^2) / 60$
45 MPH OR MORE	$L = WS$

WHERE: L=TAPER LENGTH (FT.)
 W=WIDTH OF OFFSET (FT.)
 S=POSTED SPEED LIMIT OR ANTICIPATED OPERATING SPEED (MPH)

NOTE:

MAXIMUM SPACING BETWEEN CHANNELIZING DEVICES IN A TAPER SHOULD BE APPROXIMATELY EQUAL IN FEET TO THE SPEED LIMIT

PREPARED BY: DLZ INDIANA, LLC



Brian M. Smith
2/1/2022

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CITY OF MISHAWAKA, INDIANA

ENGINEERING STANDARDS

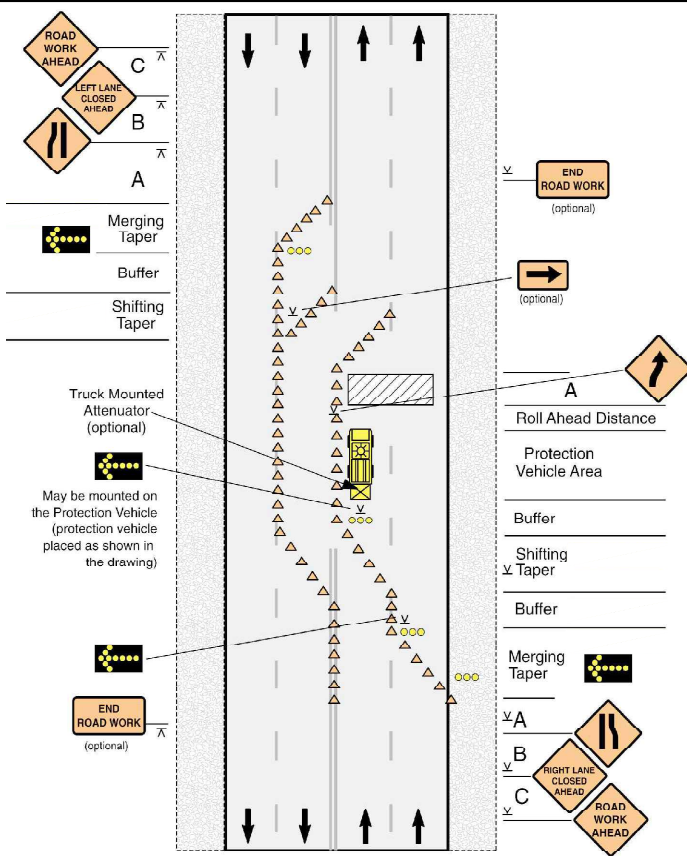
ITEM	REVISION	APPROVED DATE
	EFFECTIVE	APRIL 2007
	REVISED	FEBRUARY 2022

MAINTENANCE OF TRAFFIC

SHT. NO.
VIII-3

HALF ROAD CLOSURE ON MULTILANE ROADWAY

SHORT TERM STATIONARY – 1 TO 12 HOURS



Speed Limit (mph)	Sign Spacing A (ft)	Sign Spacing B (ft)	Sign Spacing C (ft)	Buffer (ft)
25	200	200	200	55
30	200	200	200	85
35	350	350	350	120
40	350	350	350	170
45	500	500	500	220
50	500	500	500	280
55	500	500	500	335

TAPER LENGTH FORMULA:

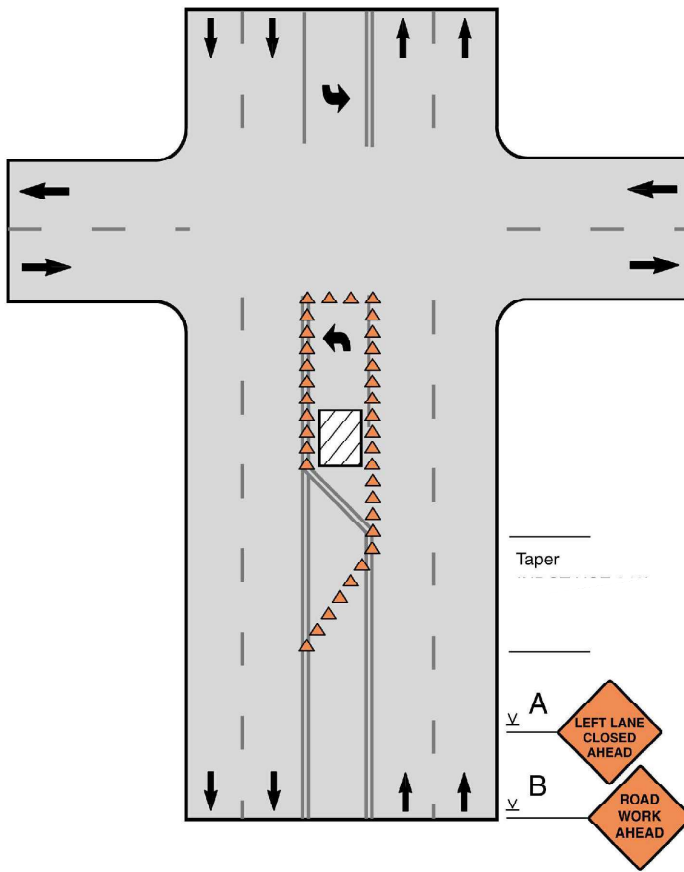
SPEED LIMIT	TAPER LENGTH (FT)
40 MPH OR LESS	$L = (WS^2) / 60$
45 MPH OR MORE	$L = WS$

WHERE: L=TAPER LENGTH (FT.)
 W=WIDTH OF OFFSET (FT.)
 S=POSTED SPEED LIMIT OR ANTICIPATED OPERATING SPEED (MPH)

NOTE:
 MAXIMUM SPACING BETWEEN CHANNELIZING DEVICES IN A TAPER SHOULD BE APPROXIMATELY EQUAL IN FEET TO THE SPEED LIMIT

TURN LANE CLOSURE AT A SIGNALIZED INTERSECTION

SHORT TERM STATIONARY – 1 TO 12 HOURS



NOTES:

1. LANE MAY BE OPENED BEYOND WORK AREA.
2. THE LENGTH OF THE TAPERS MAY BE ADJUSTED WHEN USED IN CLOSE PROXIMITY TO CROSSROADS, CURVES, OR OTHER INFLUENCING FACTORS.

PREPARED BY: DLZ INDIANA, LLC



Brian M. Smith
 2/1/2022

APPROVED/REVISED BY THE BOARD OF PUBLIC WORKS & SAFETY

ITEM	REVISION	APPROVED DATE
	EFFECTIVE	APRIL 2007
	REVISED	FEBRUARY 2022



CITY OF MISHAWAKA, INDIANA

ENGINEERING STANDARDS

MAINTENANCE OF TRAFFIC

SHT. NO.

VIII-4